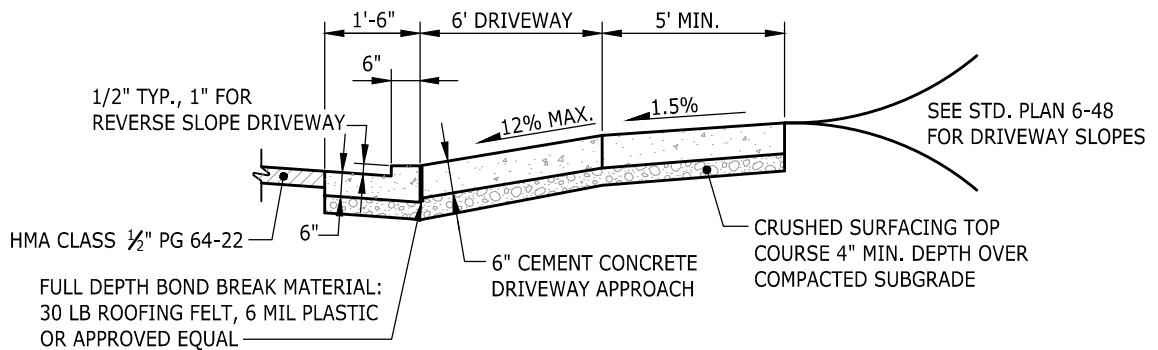


DRIVEWAY APPROACH PLAN



SECTION A-A

NOTES:

1. MAXIMUM DRIVEWAY GRADE BEHIND DRIVEWAY APPROACH IS 12%. SLOPE ROUNDING IS REQUIRED AT DRIVEWAY GRADE TRANSITIONS AS SHOWN IN SECTION A-A.
2. CONCRETE SHALL BE PORTLAND CEMENT CONCRETE MIX WITH A COMPRESSIVE STRENGTH OF 3000 PSI IN 3 DAYS. FINISH SHALL BE BRUSHED TRANSVERSELY WITH A FIBER OR WIRE BRUSH, "BROOM FINISH".
3. ALL UTILITY APPURTENANCES WHICH ALTER THE TRAVEL SURFACE SHALL NOT BE ALLOWED. DESIGN OF STORM DRAIN CATCH BASINS SHALL CONSIDER DRIVEWAY LAYOUT AND BE AVOIDED.
4. EXPANSION JOINTS SHALL BE PLACED AT 15' MAXIMUM SPACING. ELASTOMETRIC JOINT MATERIAL SHALL BE IN CONFORMANCE TO AASHTO M220.
5. ACCESS COVERS, JUNCTION BOXES, CABLE VAULTS AND OTHER APPURTENANCES WITHIN THE SIDEWALK LIMITS MUST INCLUDE A SLIP-RESISTANT SURFACE MATCHING THE GRADE OF THE SURROUNDING SIDEWALK. SUCH APPURTENANCES SHALL NOT BE PLACED WITHIN THE SLOPES OF ADA RAMPS OR DRIVEWAY WINGS.
6. PROVIDE TRANSITION SIDEWALK, 5' MIN. WHEN TRANSITIONING TO EXISTING SIDEWALK.



NOTE: THIS PLAN IS NOT A LEGAL ENGINEERING DOCUMENT BUT AN ELECTRONIC DUPLICATE. THE ORIGINAL, SIGNED BY THE ENGINEER AND APPROVED FOR PUBLICATION IS KEPT ON FILE AT THE CITY OF KENT. A COPY MAY BE OBTAINED UPON REQUEST.

		CITY OF KENT ENGINEERING DEPARTMENT	
		ALTERNATE DRIVEWAY OR PRIVATE ROAD APPROACH	
DESIGNED	COK	SCALE	NONE
DRAWN	COK	DATE	9/2020
CHECKED	COK	ENGINEER	
APPROVED		6-45	