

TRACT WIDTH TABLE

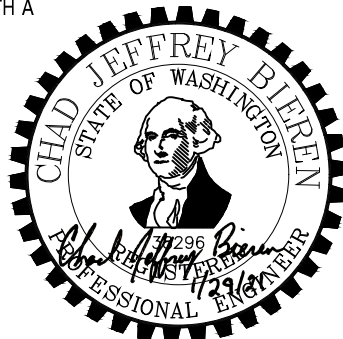
PARKING	TYPICAL LANE WIDTHS	STANDARD TRACT WIDTH
ONE SIDE ONLY	2 TRAVEL LANES AT 10' = 20' 1-6' PARKING AREA	34'
BOTH SIDES	2 TRAVEL LANES AT 10' = 20' 2-6' PARKING AREAS	40'

KDCS = KENT DESIGN AND CONSTRUCTION STANDARDS.

NOTES:

- 5' WIDE SIDEWALK/PAVED WALKWAY REQUIRED THE ENTIRE LENGTH OF A PRIVATE STREET SERVING 5 OR MORE LOTS.
- FIRE LANE MARKING PURSUANT TO 6-77.
- NO PARKING ON EITHER SIDE OF THE ROAD FOR A DISTANCE OF 15' BEFORE AND AFTER ANY FIRE HYDRANTS.
- A 1' LEVEL AREA BEHIND THE CURB OR SIDEWALK WITH A 1% MIN. AND 2% MAX. SHALL BE PROVIDED. WHEN THE SLOPE BEHIND THE 1' LEVEL AREA EXCEEDS 3:1, 0.33' OF COMPACTED CRUSHED SURFACING TOP COURSE SHALL BE PROVIDED IN THE 1' AREA.
- DESIGN SIDEWALK CROSS GRADE SHALL BE 1.5%. CONSTRUCTION TOLERANCE SHALL BE 1% MIN AND 2% MAX.
- ALL DEPTHS ARE MINIMUM COMPACTED IN-PLACE DIMENSIONS.
- SOIL STABILIZATION FABRIC MAY BE REQUIRED BY THE ENGINEER; PRIOR TO THE PLACEMENT OF GRAVEL BORROW. WHEN REQUIRED THE CONTRACTOR SHALL PLACE A NON-WOVEN GEOTEXTILE FABRIC OVER THE PREPARED SUBGRADE WITH A MINIMUM 2 FOOT OVERLAP.
- ASPHALT TEMPERATURE SHALL NOT EXCEED 325° AT THE DISCHARGE PLANT. INITIAL BREAKDOWN ROLLING AND COMPACTIVE REPORT SHALL OCCUR PRIOR TO THE INTERNAL MAT TEMPERATURE REACHING 225°. FINAL COMPACTION DENSITY SHALL BE REACHED PRIOR TO THE INTERNAL MAT TEMPERATURE BEING 185°.
- THE MAXIMUM COMPACTED THICKNESS OF ANY SINGLE ASPHALT LIFT SHALL MEET THE REQUIREMENTS OF WSDOT STD. SPEC. 5-04.3(9), WITH WEATHER LIMITATIONS OUTLINED IN 5-04.3(1) CONSIDERED.
- THE FACE OF ALL CURB AND GUTTER ABUTTING ASPHALT LIFTS SHALL RECEIVE A UNIFORM BRUSH APPLIED TACK COAT, CSS OR EQUIVALENT. ALL MEET LINES BETWEEN LIFTS SHALL BE UNIFORM AND VERTICAL. THE MEET LINES SHALL BE CLEAN AND TACK COATED. WHEN SUCCESSFUL LIFTS ARE PLACED A TACK COAT SHALL BE UNIFORMLY APPLIED BY A PROPERLY HEATED AND MAINTAINED MECHANICAL DISTRIBUTOR. LONGITUDINAL PAVEMENT JOINTS SHALL OVERLAP A MINIMUM OF 2 INCHES, TYPICAL.

NOTE: THIS PLAN IS NOT A LEGAL ENGINEERING DOCUMENT BUT AN ELECTRONIC DUPLICATE. THE ORIGINAL, SIGNED BY THE ENGINEER AND APPROVED FOR PUBLICATION IS KEPT ON FILE AT THE CITY OF KENT. A COPY MAY BE OBTAINED UPON REQUEST.



	CITY OF KENT ENGINEERING DEPARTMENT	
	PRIVATE STREET	
DESIGNED: COK DRAWN: COK CHECKED: COK	SCALE: NONE DATE: 9/2020	STANDARD PLAN 6-15
APPROVED:	ENGINEER	